



**US Army Corps
of Engineers®**

U.S. Department of
Homeland Security

United States
Coast Guard



Commander

Ninth Coast Guard District

PUBLIC NOTICE

Applicant:
Chicago Department of Transportation

Published: June 27, 2025
Expires: July 27, 2025

**Chicago District
Regulatory Permit Application No. LRC-2011-685
Section 408 Permit Application No. 408-LRC-2021-0018
Coast Guard Public Notice No. 09-04-25**

TO WHOM IT MAY CONCERN: This Joint Public Notice (JPN) is issued on behalf of the U.S. Army Corps of Engineers (Corps) Chicago District and the Commander, Ninth Coast Guard District (USCG), who have each received applications regarding the proposed replacement of the Division Street Bridge across mile 3.30 of the North Branch of the Chicago River in Chicago, Cook County, Illinois.

Corps:

The Corps has received an application dated June 25th, 2024, for a Department of the Army permit pursuant to Section 404 of the Clean Water Act (33 U.S.C. §1344). The Corps is also evaluating a Section 408 (33 U.S.C. § 408) permission under Section 14 of the Rivers and Harbors Act of 1899 to alter the Chicago River Navigation project.

USCG:

The USCG reviews bridge location and plans under the authority of several acts pertaining to bridges. These acts include, but are not limited to, Section 9 of the Rivers and Harbors Act of 1899 (33 §§ U.S.C. 401, 502) and the General Bridge Act of 1946 (33 §§ U.S.C. 525-533). The USCG has received application materials, dated June 7, 2024 for approval of the location and plans for a permanent replacement for the Division Bridge as well as for construction of a temporary fixed bridge located immediately north of the existing Division Street Bridge.

The purpose of this public notice is to solicit comments from the public regarding the work described below. In particular, the Corps of Engineers and USCG would appreciate any comments on the potential impacts of a manually-liftable bridge across the North Branch of the Chicago River in terms of commercial and recreational navigation (present and future) in the Federal channel north of Division Street. The views of mariners, owners of maritime facilities and other businesses, environmental organizations, and other river stakeholders will be especially valuable in assessing this requested action.

APPLICANT: Soliman Khudeira
Chicago Department of Transportation
2 N. LaSalle Street, Suite 820
Chicago, IL 60602

AGENT: Krashan Singhal
WSP USA
30 N LaSalle Street, Suite 4200
Chicago, IL 60602

WATERWAY AND LOCATION: The project would affect the North Branch Chicago River, a navigable water of the United States. The project/review area is located along Division Street, approximately 0.03 miles east of the intersection of Division Street and N Elston Avenue in Section 5, 039N Township, 014E Range; at Latitude 41.903492 and Longitude -87.65741; in Chicago, Cook County, Illinois.

EXISTING CONDITIONS: The current Division Street Bridge across the North Branch of the Chicago River is a double-leaf, trunnion type movable bascule movable bridge that was constructed in 1904 by the City of Chicago Department of Public Works and rehabilitated by the City of Chicago in 1992. The existing bridge is designed to carry two lanes of traffic. The east and west approaches to the bridge carry four lanes of traffic which taper abruptly at the bridge.

The North Branch Chicago River is navigable. It has an unconsolidated bottom and highly altered banks within and adjacent to the project area. It is currently in use by commercial and recreational navigation.

PROJECT PURPOSE:

Basic: To allow surface transportation across the North Branch of the Chicago River.

Overall: The proposed bridge will be designed to allow four lanes of traffic on the bridge thereby eliminating the bottleneck to traffic due to the existing bridge. A manually liftable bridge is proposed to balance the needs of navigation with the history of infrequent bridge lifts at this location.

PROPOSED WORK: CDOT proposes to replace the existing movable bridge with a movable bridge. The proposed movable bridge would be constructed without lifting machinery. However, as described below, it would be constructed in a manner that it could be lifted by a sequence of manual jacking and shoring. Alternatively, permanent lifting machinery could be installed if CDOT were directed to do so. CDOT also proposes to construct a temporary fixed bridge located immediately north of the location of the existing Division Street Bridge. The temporary bridge would be used to detour Division Street traffic during the removal of the existing Division Street bridge and construction of its replacement. The temporary bridge would be removed upon completion of construction of the permanent movable replacement for the Division Street Bridge.

The existing structure is a double-leaf, trunnion type bascule bridge. The existing bridge's superstructure would be completely removed. The top of the existing river piers and anchor piers would be partially removed for the installation of components of

the permanent replacement movable bridge as well as for potential future installation of a riverwalk. The applicant proposes to replace the existing structure with a tensioned tied-arch structure. The proposed replacement movable bridge would be designed and constructed in a manner that it would be initially built without lifting machinery. However, the design and construction of the proposed replacement bridge would allow it to be opened manually through a sequence of jacking and shoring with up to 21-day to 30-day notice by Commander, Ninth Coast Guard District. The Applicant has requested to be able to provide 30-days notice. Both the Corps and USCG prefer 21-days or less of notice and seek the input of mariners as to what notice would meet the reasonable current and prospective needs of navigation. Comments received in response to this public notice, or lack thereof, would aid in making that determination. This jacking and shoring would be conducted by the applicant and contracted personnel utilizing equipment owned or sourced by the contractor. Once lifting equipment and personnel are brought to the bridge, it would take approximately 24-hours to lift or lower the bridge via manual jacking and shoring. Additionally, CDOT would, upon direction by Commander, Ninth Coast Guard District, install permanent lifting machinery and bring the bridge to full operation in coordination with the USCG.

The need to have a movable bridge at this location is based upon the past, present, and future needs of navigation. The Coast Guard issued regulations at 33 C.F.R. 117.391(c) in November 1995 and May 2002 that have allowed the existing bridge and the following additional bridges located on the North Branch near it to not open for the passage of vessels: the North Avenue bridge across mile 3.81, the Cortland Avenue across mile 4.48, the Webster Avenue bridge across mile 4.85, the North Ashland Avenue bridge across mile 4.90, and the Union Pacific Railroad across mile 5.01, over North Branch of Chicago River. Similarly, the Coast Guard allowed the construction of a temporary fixed Chicago Avenue bridge across mile 2.40 by regulation in June 2019 while the City of Chicago sought application for a replacement movable bridge. There have not been openings for these bridges since approximately 1982. The North Branch of the Chicago River is currently utilized by large commercial tug and barge traffic, passenger vessels, and powered and unpowered recreational vessels. These vessels are able to pass through the existing bridge as well as the other bridges mentioned here without an opening. However, as noted by the Coast Guard during the issuance of these regulations, these movable bridges may be required to be made operational again within a reasonable time if ordered by the Coast Guard in the future. This caveat reflects a recognition of the dynamic needs on the waterway tied to maritime commerce within the Chicago Area as well as on the Great Lakes and globally. The existing regulation at 33 C.F.R. 117.391(c) would transfer to the proposed permanent replacement bridge.

CDOT proposes to first construct the proposed temporary bridge north of the existing structure in addition to temporary road lanes diverting Division Street traffic. Once completed, CDOT would undertake the removal of portions of the existing structure and construction of the proposed replacement structure. Though final means and methods have not been determined, it is anticipated that construction and removal of the temporary run-around bridge, removal of portions of the existing structure, and

construction of the proposed replacement structure may utilize both shore-based and in-water equipment. In-water work could include the use of barges or sediment control measures. Any sediment control measures would not encroach into the navigation channel. Similarly, any barges or equipment that would be used for removal or construction of the existing and proposed bridges would be operated in a manner that they could clear the navigable channel with advance notice from mariners. CDOT and its contractors would work with the USCG, who will coordinate any construction with mariners in a manner to ensure the continuity of navigation.

The project proposes increases in the bridge foundation footprint to accommodate the proposed bridge structure, leading to the discharge of fill into a 0.02-acre area within the river. Proposed temporary construction activities include the installation of a cofferdam over a 0.004-acre portion of the river for relocation of a sewer outfall. This cofferdam would likewise not encroach into the navigation channel.

One existing navigational dolphin is proposed to be replaced in kind as part of the project.

Navigational clearances of the existing and proposed temporary and permanent replacement structures are described in the NAVIGATIONAL CLEARANCES section below.

AVOIDANCE AND MINIMIZATION: The applicant has provided the following information in support of efforts to avoid and/or minimize impacts to the aquatic environment: "Avoidance of these minimal impacts is not possible since a new permanent bridge is needed at this location. The terms and conditions of the Nationwide Permit Program will be followed, which will minimize impacts to the River."

While the project is no longer being reviewed under the referenced nationwide permits, it was still designed to meet the terms and conditions of those permits in order to avoid and minimize impacts to WOTUS to the maximum extent practicable.

LEAD FEDERAL AGENCY: The Federal Highway Administration is the lead Federal agency for satisfying the requirements of the National Environmental Policy Act (NEPA). The FHWA is acting on behalf of the USCG for all environmental control laws. A Categorical Exclusion was issued on dated February 26, 2025, pursuant to NEPA, as amended. The USCG has tentatively determined that the proposed action will not have a significant impact and anticipates issuing a Categorical Exclusion for the purposes of NEPA.

COMPENSATORY MITIGATION: The applicant has provided the following explanation why compensatory mitigation should not be required: "Mitigation for wetland impacts is not proposed per previous project direction."

CULTURAL RESOURCES: The Illinois Department of Transportation (IDOT) reviewed the project on behalf of the Federal Highway Administration (FHWA) for compliance with Section 106 of the National Historic Preservation Act (NHPA). The District Engineer's

final eligibility and effect determination will be based upon coordination with IDOT and the State Historic Preservation Office, as appropriate and required, and with full consideration given to the proposed undertaking's potential direct and indirect effects on historic properties within the permit area.

ENDANGERED SPECIES: IDOT reviewed the project on behalf of the FHWA for compliance with Section 7 of the Endangered Species Act (ESA) and made the following determinations:

- Northern long-eared bat (*Myotis septentrionalis*) – No Effect
- Piping plover (*Charadrius melodus*) – No Effect
- Rufa Red knot (*Caladris canutus rufa*) – No Effect
- Whooping Crane (*Grus americana*) – No Effect
- Eastern Massasauga (*Sistrurus catenatus*)- No Effect
- Hine's Emerald Dragonfly (*Somatochlora hineana*)- No Effect
- Monarch butterfly (*Danaus plexippus*) – No Effect
- Eastern Prairie Fringed Orchid (*Platanthera Leucophaea*)-No Effect
- Leafy Prairie-clover (*Dalea foliosa*)- No Effect

No additional consultation with the USFWS is requested at this time.

NAVIGATION: The proposed structure or activity would affect the Chicago River Navigation Project, specifically the deep-draft (21 ft. depth) portion authorized by the Rivers and Harbors Acts of 1896, 1899, 1907, & 1919.

NAVIGATIONAL CLEARANCES: The proposed temporary bridge and permanent replacement bridge would have clearances as described in the table below. The proposed clearances for both the proposed permanent movable bridge in the closed position as well as the proposed temporary fixed bridges would result in an increase from current minimum vertical navigational clearances. The proposed permanent replacement movable bridge would result in a reduced vertical clearance in the open position that would still meet the reasonable needs of navigation. Both the proposed temporary fixed bridge and permanent replacement movable bridges would provide the same horizontal navigational clearance as the existing bridge.

	Existing Movable	Proposed Temporary Fixed	Proposed Permanent Movable Replacement
Vertical (Closed)	18.70-feet throughout	22.46-feet throughout	19.34-feet throughout
Vertical (Open)	Unlimited (though currently not required to open by regulation)	N/A	40.00-feet throughout

	Existing Movable	Proposed Temporary Fixed	Proposed Permanent Movable Replacement
Horizontal	154.75-feet face-to-face of existing piers, with 100.00-feet between dolphins normal to the axis of the channel as the navigation channel.	147.07-feet face-to-face of existing piers, with 100.00-feet between dolphins normal to the axis of the channel as the navigation channel.	154.75-feet face-to-face of existing piers, with 100.00-feet between dolphins normal to the axis of the channel as the navigation channel.

Datum: All elevations referenced to Low Water Datum, elevation 576.67-feet, International Great Lakes Datum of 1985 (IGLD 85).

Map of location and plans are attached.

SECTION 408: The applicant will require permission under Section 14 of the Rivers and Harbors Act of 1899 (33 USC 408) because the activity, in whole or in part, would alter, occupy, or use a Corps Civil Works project.

WATER QUALITY CERTIFICATION: Water Quality Certification (WQC) is required from the Illinois Environmental Protection Agency (IEPA). The applicant applied for WQC on August 27, 2024 (IEPA log number C-0177-24). The IEPA issued a WQC on 18 December 2024.

COASTAL ZONE MANAGEMENT: The applicant made a determination that the project is consistent with the State of Illinois' coastal management program, and the Illinois Department of Natural Resources Coastal Management Program (IDNR CMP) concurred with this determination on 14 October 2024.

FLOODPLAINS: The bridge is located in a Federal Emergency Management Agency (FEMA) Zone A Special Flood Hazard Area (SFHA) without a base flood elevation (BFE). The 100-year flood elevation as indicated on the plan sheets for the proposed permanent replacement movable bridge is 582.58-feet. The elevation of the low member of the navigation span is 596.01-feet. The 100-year flood elevation as indicated on plan sheets for the proposed temporary fixed bridge is 583.58-feet. The elevation of the low member of the navigation span is 596.75-feet. Elevations are referenced to IGLD 85 datum. Based upon the information provided in the Applicant, approximately 187.70 cubic yards of fill material will be placed below mean high water level for the construction of the bridge and approaches.

NOTE: This public notice is being issued based on information furnished by the applicant. This information has not been verified or evaluated to ensure compliance with

laws and regulation governing the regulatory program. The geographic extent of aquatic resources within the proposed project area that either are, or are presumed to be, within the Corps jurisdiction has been verified by Corps personnel.

CORPS EVALUATION: The decision whether to issue a permit will be based on an evaluation of the probable impact including cumulative impacts of the proposed activity on the public interest. That decision will reflect the national concern for both protection and utilization of important resources. The benefits, which reasonably may be expected to accrue from the proposal, must be balanced against its reasonably foreseeable detriments. All factors which may be relevant to the proposal will be considered including cumulative impacts thereof; among these are conservation, economics, esthetics, general environmental concerns, wetlands, historical properties, fish and wildlife values, flood hazards, floodplain values, land use, navigation, shoreline erosion and accretion, recreation, water supply and conservation, water quality, energy needs, safety, food, and fiber production, mineral needs, considerations of property ownership, and in general, the needs and welfare of the people. Evaluation of the impact of the activity on the public interest will also include application of the guidelines promulgated by the Administrator, EPA, under authority of Section 404(b) of the Clean Water Act (CWA) or the criteria established under authority of Section 102(a) of the Marine Protection Research and Sanctuaries Act of 1972. A permit will be granted unless its issuance is found to be contrary to the public interest.

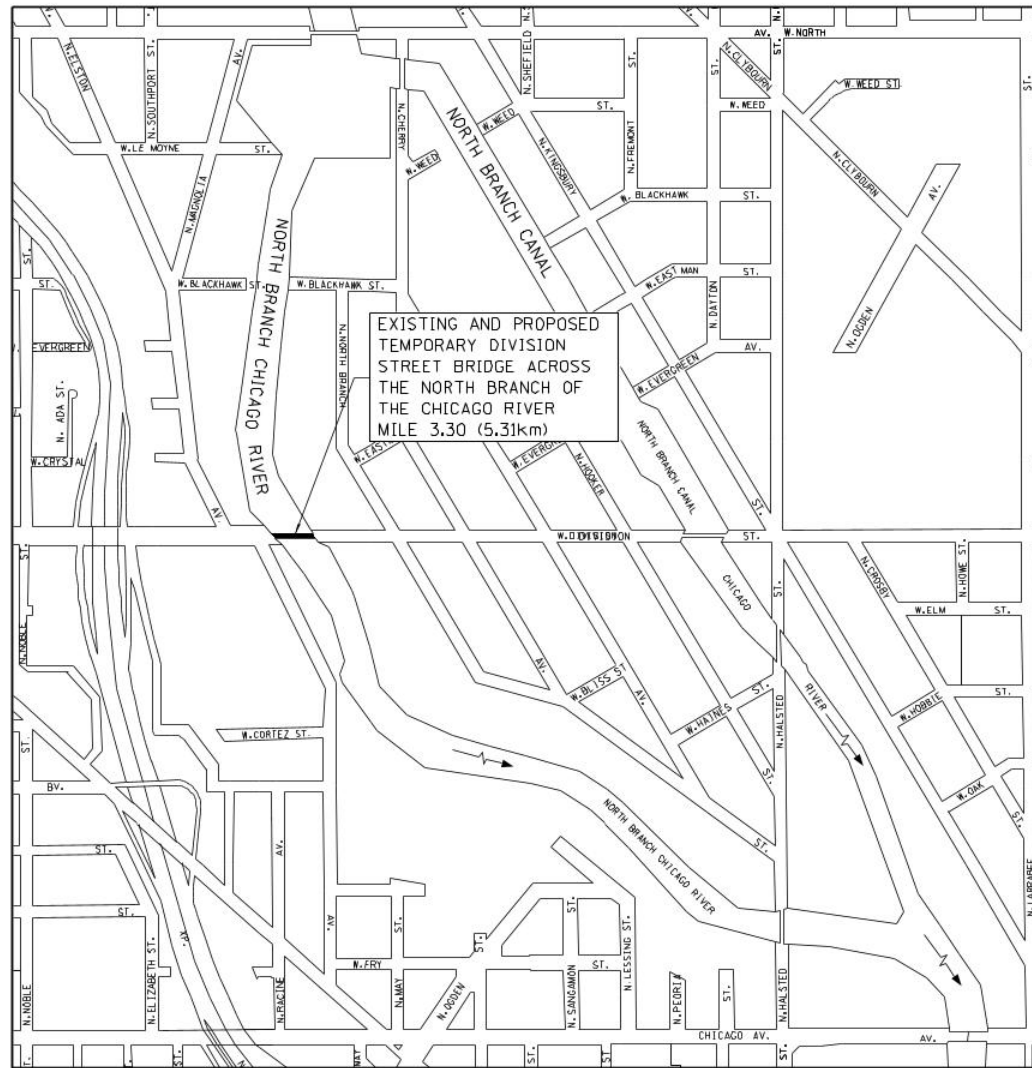
USCG EVALUATION: The USCG will determine whether to issue a permit based upon whether the project will meet the reasonable needs of navigation.

COMMENTS TO CORPS: The Corps is soliciting comments from the public; Federal, State, and local agencies and officials; Indian Tribes; and other Interested parties in order to consider and evaluate the impacts of this proposed activity. Any comments received will be considered by the Corps to determine whether to issue, modify, condition, or deny a permit for this proposal. To make this determination, comments are used to assess impacts to endangered species, historic properties, water quality, general environmental effects, and the other public interest factors listed above. Comments are used in the preparation of an Environmental Assessment (EA) and/or an Environmental Impact Statement pursuant to the National Environmental Policy Act (NEPA). Comments are also used to determine the need for a public hearing and to determine the overall public interest of the proposed activity.

The Chicago District will receive written comments on the proposed work, as outlined above, until July 27, 2025. Comments should be submitted electronically via the Regulatory Request System (RRS) at <https://rrs.usace.army.mil/rrs> or to Erich Ceisel at Erich.M.Ceisel@usace.army.mil. Alternatively, you may submit comments in writing to the Commander, U.S. Army Corps of Engineers, Chicago District, Attention: Erich Ceisel, 231 South LaSalle Street, Chicago, IL 60604. Please refer to the permit application number in your comments.

Any person may request, in writing, within the comment period specified in this notice, that a public hearing be held to consider the application. Requests for public hearings shall state, with particularity, the reasons for holding a public hearing. Requests for a public hearing will be granted, unless the District Engineer determines that the issues raised are insubstantial or there is otherwise no valid interest to be served by a hearing.

COMMENTS TO USCG: Mariners and waterway users – including marine facilities and other interested parties – are requested to comment, in writing, regarding the proposed navigation clearances, the need for clearance gauges, navigational lighting, decorative lighting, and operating schedules or regulations. Comments should be of sufficient detail to establish a clear understanding of reasons for support or opposition to the proposed project. We will forward comments of an environmental nature such as those regarding wildlife refuges, public parks, historic sites, wetlands, floodplain issues, air, water quality, environmental justice, etc. to the FHWA. Comments to USCG can be electronically submitted to Michael.O.Walker2@uscg.mil. If electronic submittal is not available, commenters should address replies to: Commander, Ninth Coast Guard District (dpb), 1240 E. 9th Street, Cleveland, OH 44199



LOCATION MAP

NOTES:

1. PUBLIC WILDLIFE AND WATERFOWL REFUGES, PARK AND RECREATION AREAS, AND ARCHAEOLOGICAL SITES ARE NOT LOCATED IN THE VICINITY OF THE PROJECT.
2. THERE ARE NO HISTORIC BUILDINGS ADJACENT TO THE BRIDGE WHICH WILL BE IMPACTED BY THE REPLACEMENT OF THE BASCULE BRIDGE WITH A NEW TEMPORARY FIXED BRIDGE.
3. PROJECT IS WITHIN THE CITY LIMITS OF CHICAGO.
4. ELEVATIONS SHOWN ON THESE PLANS ARE BASED ON INTERNATIONAL GREAT LAKES DATUM OF 1985 (IGLD 1985).



Date of Signature: 08/08/2024
License Expiration Date: 11/30/2026

Conceptual plans utilized to obtain Coast Guard bridge permit.



VICINITY MAP



CITY OF CHICAGO DEPARTMENT OF TRANSPORTATION DIVISION OF ENGINEERING		
DIVISION ST ACROSS THE NORTH BRANCH OF THE CHICAGO RIVER PROPOSED SN 016-6210 MILE POST 3.30 CITY OF CHICAGO, COOK COUNTY, ILLINOIS		
VICINITY AND LOCATION MAP		
8/5/2024	SHEET 1 OF 6	HNTB Corporation

☐ W. Abut. &
☐ Drilled
Shafts

240.50' Bk. to Bk. Abutments

☐ E. Abut. &
☐ Drilled
Shafts

☐ Pier 1 Arch Brg. &
☐ Drilled Shafts

☐ Pier 2 Arch Brg. &
☐ Drilled Shafts

☐ Exp. Jt.

Existing Steel
Dolphin to be replaced

Existing Steel
Dolphin to remain

100.00' Clear
Prop. Channel Width

☐ Control
Channel

Exist. Steel
Dolphins
to remain

Existing Dock
Wall

☐ Exp. Jt.

N

McGrath
Acura

COMMERCIAL RETAIL
STRIP PLAZA BY
1200 PARTNERS, LLC

Bk. W. Abut.

1.00'
typ.

90.38' Out to Out

☐ Chicago River =
☐ W. Division Street

Existing Steel Dolphin
to remain, typ.
☐ Existing Bridge
Superstructure
(to be removed entirely)

Bk. E. Abut.

ABANDONED 12"

ESTATE
RESTAURANT

Exist. R.O.W.

☐ Existing
River Pier
to be partially
removed

7.50'
Sidewalk
Edge of Bridge
Existing Steel Dolphins
to remain
Existing Dock Wall

Flow
North Branch of
the Chicago
River

☐ Existing River
Pier to be
partially removed

Existing Steel
Dolphins to remain

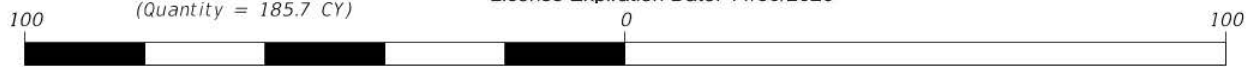
McGrath
Lexus

LEGEND

- Permanent Cofferdam
- Prop. Light Pole
- Existing Structure
- Water Depth from Design Low Water Elevation
- Porous Granular Embankment (foundation fill within waterway) (Quantity = 185.7 CY)



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SCALE IN FEET

PLAN

NOTES:

- All elevations are based on IGLD (International Great Lakes Datum) of 1985.

CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING

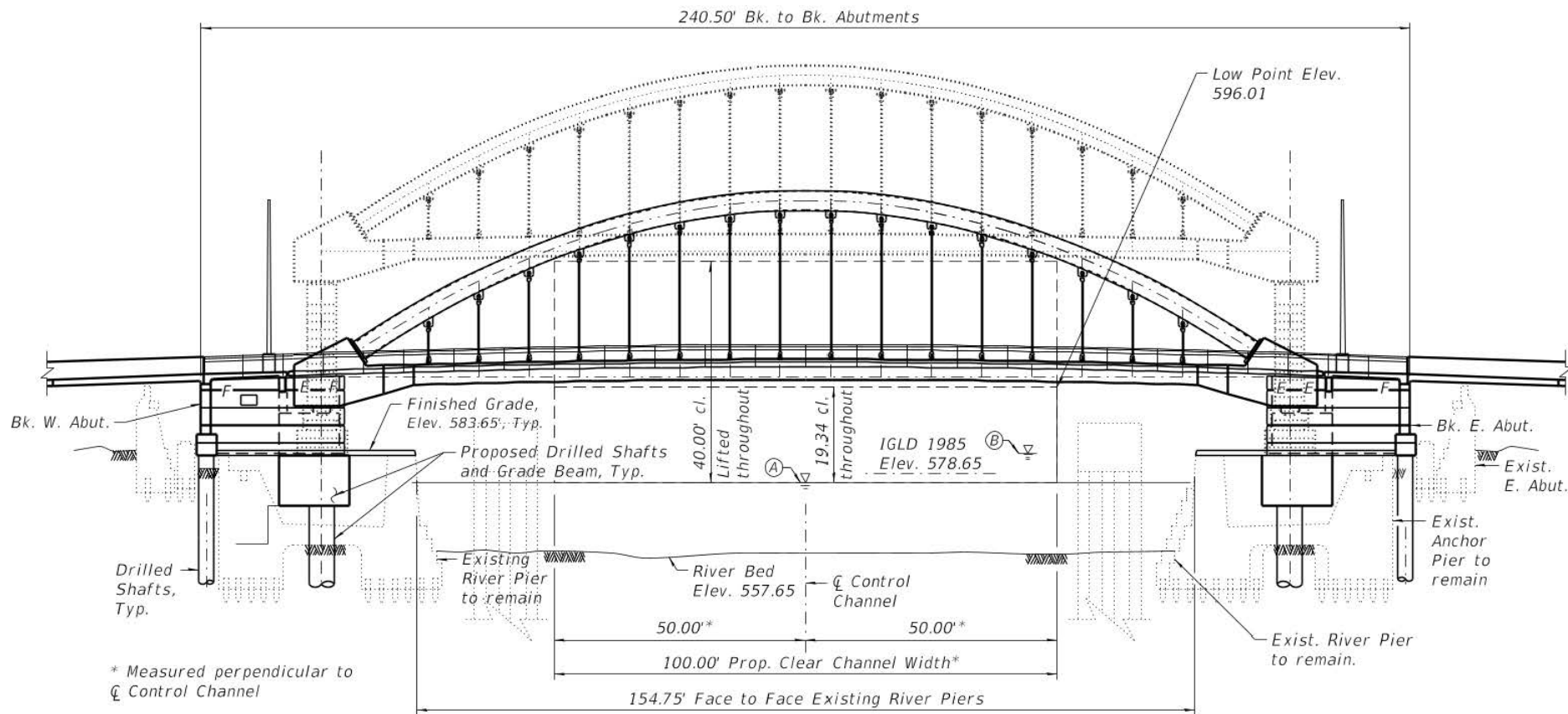
DIVISION ST ACROSS
THE NORTH BRANCH OF THE CHICAGO RIVER
PROPOSED SN 016-6210
MILE POST 3.30
CITY OF CHICAGO, COOK COUNTY, ILLINOIS

BRIDGE GENERAL PLAN

8/5/2024

SHEET 2 OF 6

HNTB Corporation



ELEVATION

WATER ELEVATION TABLE

Loc.	Elev.	Description
A	576.67	Design Low Water
B	582.58	100 Year Flood

All elevations are based on IGLD (International Great Lakes Datum) of 1985



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SCALE IN FEET

CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING

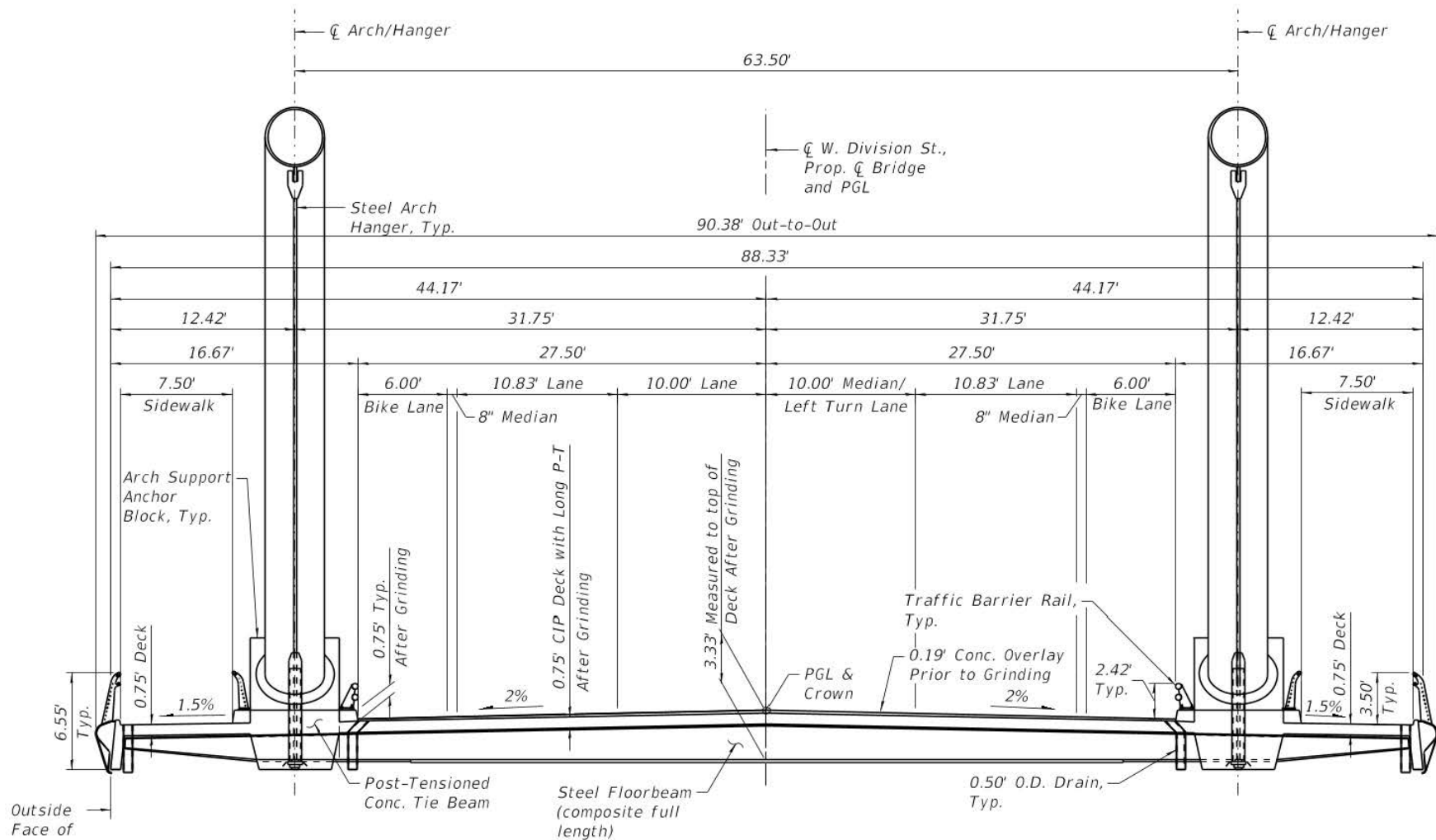
DIVISION ST ACROSS
THE NORTH BRANCH OF THE CHICAGO RIVER
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BRIDGE ELEVATION

8/5/2024

SHEET 3 OF 6

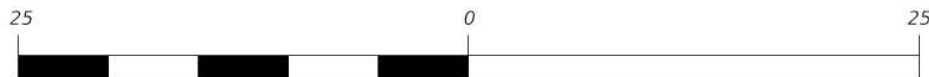
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SECTION THRU MAIN SPAN



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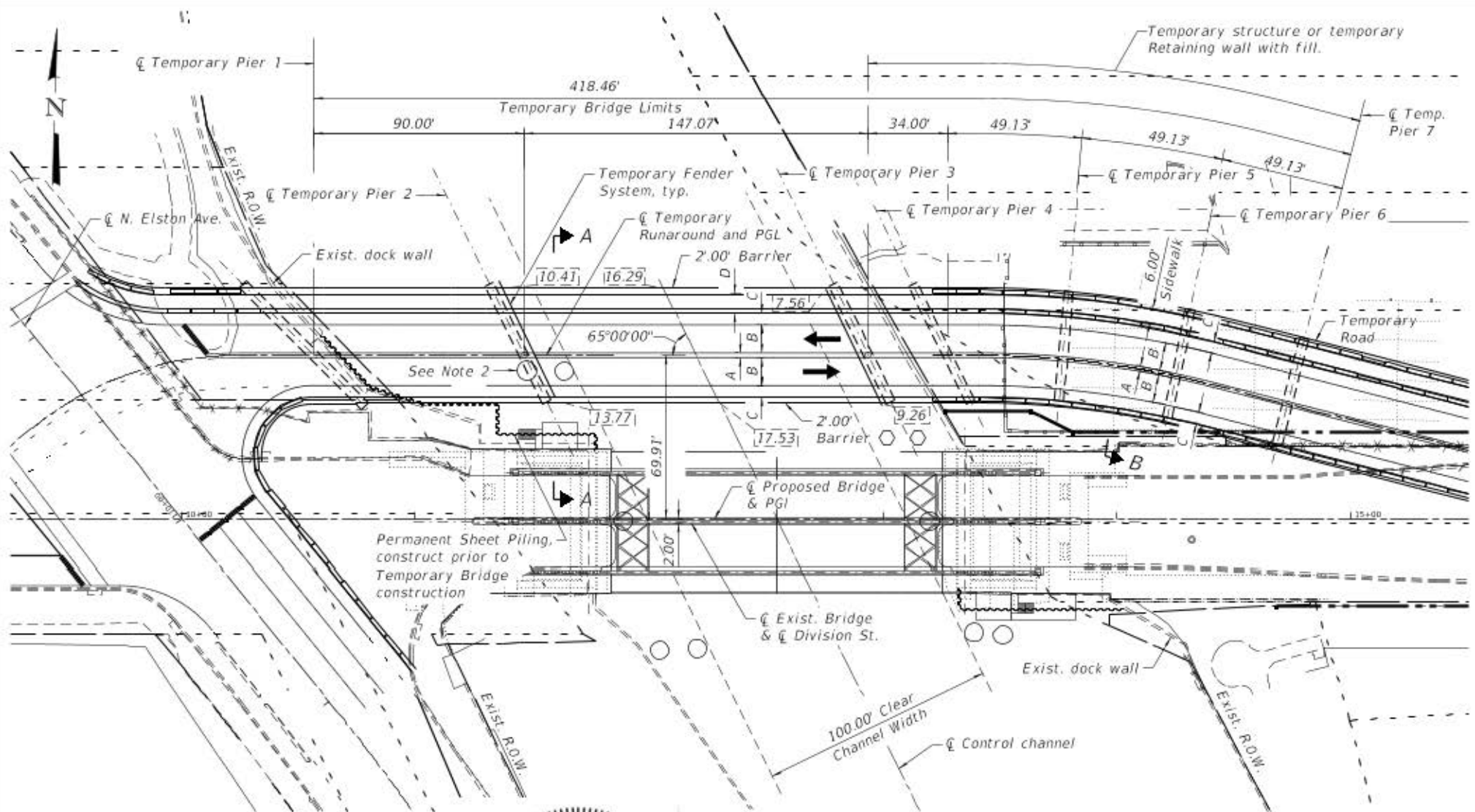
CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING

DIVISION ST ACROSS
THE NORTH BRANCH OF THE CHICAGO RIVER
PROPOSED SN 016-6210
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CITY OF CHICAGO, COOK COUNTY, ILLINOIS




8/5/2024

SHEET 4 OF 6

HNTB Corporation



LEGEND:

-  Permanent Cofferdam
-  Existing Structure
-  Water Depth from Design Low Water Elevation

NOTES:

1. For Section A-A and Section B-B, see Sheet 6.
2. Remove existing steel dolphin to construct temporary pier. Replace removed dolphin with new pier protection cell after the temporary pier is removed.
3. All elevations are based on IGLD (International Great Lakes Datum) of 1985.



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100 0 100

SCALE IN FEET

PLAN

**CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING**

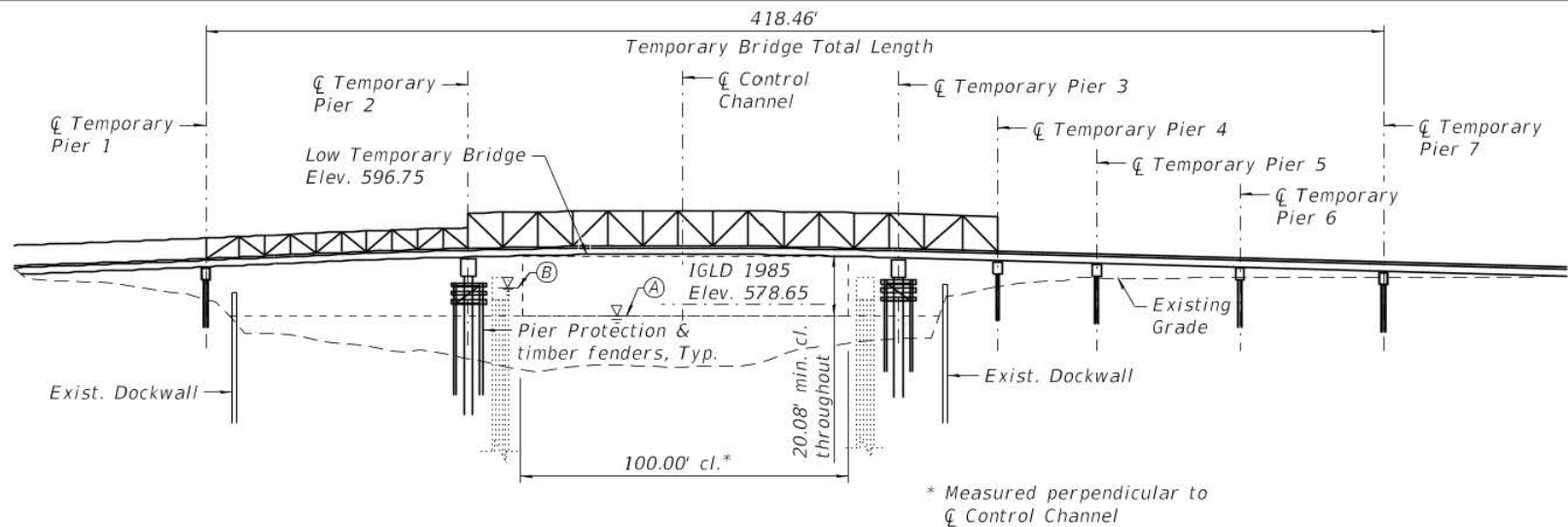
**DIVISION ST ACROSS
THE NORTH BRANCH OF THE CHICAGO RIVER
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CITY OF CHICAGO, COOK COUNTY, ILLINOIS**

TEMPORARY BRIDGE GENERAL PLAN

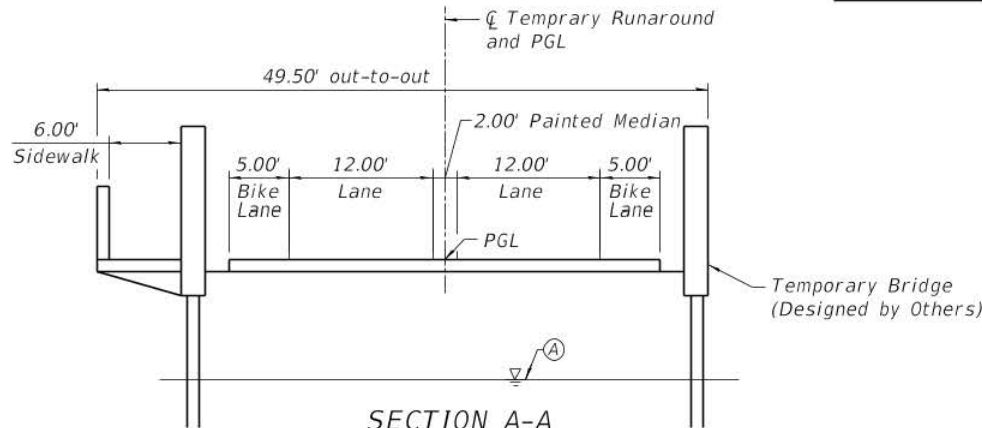
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SHEET 5 OF 6

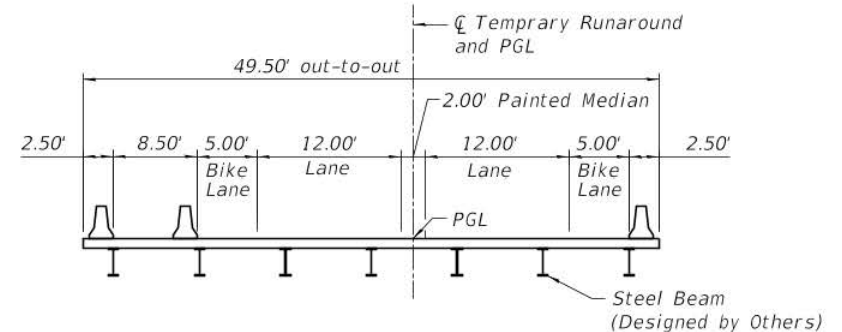
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ELEVATION



SECTION A-A
TEMPORARY BRIDGE

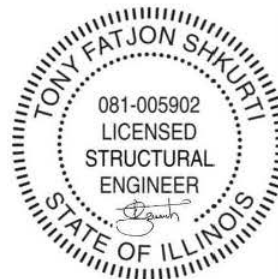


SECTION B-B
TEMPORARY BRIDGE

WATER ELEVATION TABLE

Loc.	Elev.	Description
A	576.67	Design Low Water
B	583.58	100 Year Flood

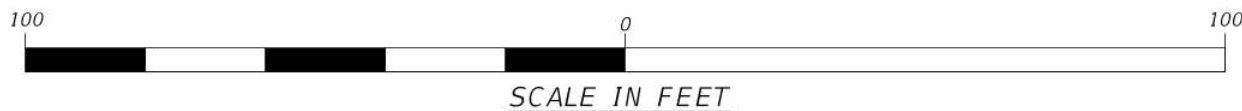
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NOTES:

- For location of Section A-A and Section B-B, see Sheet 5.



CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING

DIVISION ST ACROSS
THE NORTH BRANCH OF THE CHICAGO RIVER
PROPOSED SN 016-6210
MILE POST 3.30
CITY OF CHICAGO, COOK COUNTY, ILLINOIS

TEMP. BRIDGE ELEVATION AND SECTIONS

8/5/2024

SHEET 6 OF 6

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