



23 March 2022

PUBLIC NOTICE (02-22)

The Commander, Thirteenth Coast Guard District has received a request for a Preliminary Navigation Clearance Determination (PNCD) from the Interstate Bridge Replacement (IBR) Program aka Portland to Vancouver I-5 twin bridges. The Oregon Department of Transportation (ODOT) and the Washington State Department of Transportation (WSDOT) are leading the IBR Program efforts with the United States Coast Guard (USCG) on the PNCD. In addition to ODOT and WSDOT, the IBR Program Team is also comprised of the local transit agencies (C-Tran and TriMet), and the regional Metropolitan Planning Organization, Metro. These agencies together are the IBR Program proponents responsible for administering the IBR Program. A PNCD is the first step in the U.S. Coast Guard Bridge permitting process and defines the bridge clearances which have been evaluated and determined to have a high likelihood of being approved by the Coast Guard and to help the applicant refine development of alternatives for a proposed bridge.

At the request of the Coast Guard the IBR team has prepared a navigation impact report (NIR) for the proposed IBR. Based off an analysis of the NIR and Public Comments the Coast Guard will prepare a PNCD. This Public Notice (PN) is soliciting for comments exclusively related to navigation. Maritime transportation system stakeholders (vessels and facilities) are highly encouraged to carefully review this notice and provide comments with regard to the proposed bridge's ability to meet the needs of navigation to include mariner requirements for horizontal navigation clearances and vertical navigation clearances, including air draft and air gap requirements (see diagram below) The Coast Guard is particularly interested in receiving comments from maritime stakeholders with current and or future vertical navigation clearance requirements of greater than 116 feet (air draft and air gap).

WATERWAY AND LOCATION: Columbia River, river mile 106.5, between Portland, OR. and Vancouver, WA.

CLEARANCE DETERMINATION: The IBR project proposes the replacement of the current Portland to Vancouver twin I-5 Highway Drawbridges (LLNR 11258) with a fixed bridge with a significant (32%) reduction in vertical navigation clearances as compared to the existing bridge. The existing and proposed clearances are listed below:

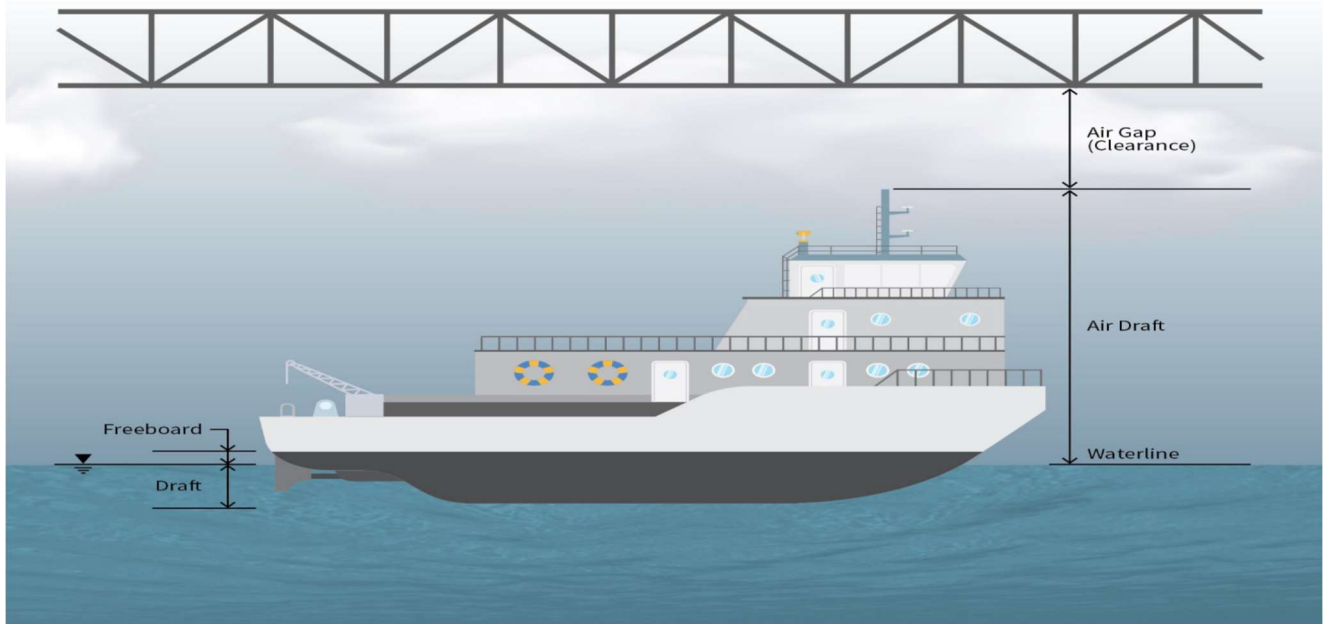
NAVIGATIONAL CLEARANCE (See note 1, and diagrams below)	<u>Existing</u>	<u>Proposed</u>
Horizontal	Main channel – 283 ft. Barge – 511 ft. Alt Barge – 260 ft.	North channel - 300 ft. Middle channel - 300 ft. South channel - 300 ft.
Vertical	Main channel raised – 178 ft. Main channel lowered – 39 ft. Barge – 46 ft., 53 ft., 70 ft. Alt Barge – 72 ft.	North channel - 99 ft. Middle channel - 116 ft. South channel - 113 ft.

Note 1: Vertical clearances measured above 0 Columbia River Datum (CRD).

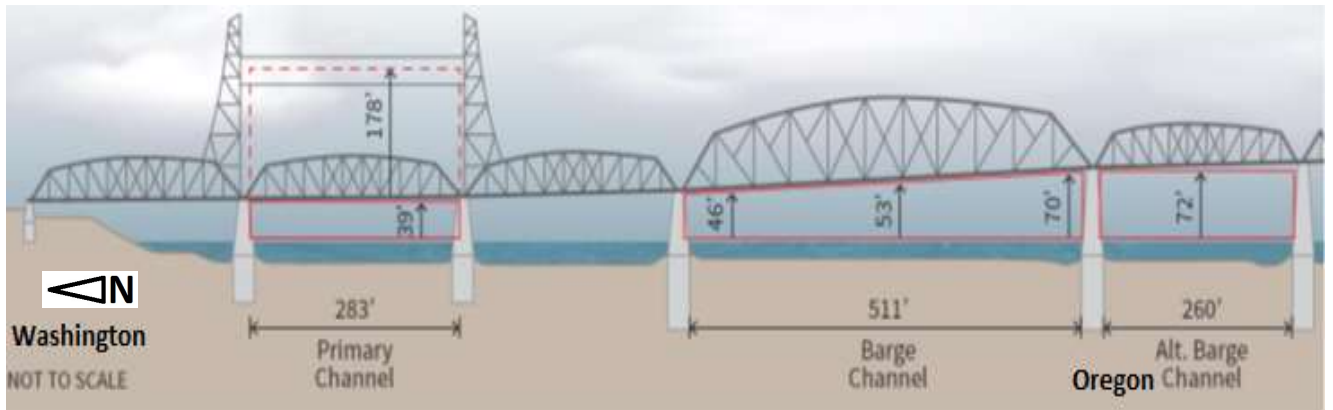
Existing Columbia River Navigation Clearances from the Pacific Ocean to river mile 106.5

Bridge	River Mile	Horizontal Clearance (feet)	Vertical Clearance (feet)	Vertical Clearance with Span Open (feet)
Astoria-Megler Bridge	13.5	1,070	193	NA
Power Cable	40.0	NA	230	NA
Power Cable	62.4	NA	216	NA
Lewis & Clark Bridge	66.0	1,120	187	NA
Power Cable	104.2	NA	220	NA
BNSF Rail Bridge	105.6	200	39	Unlimited
Existing Interstate Bridge	106.5	263	39	178

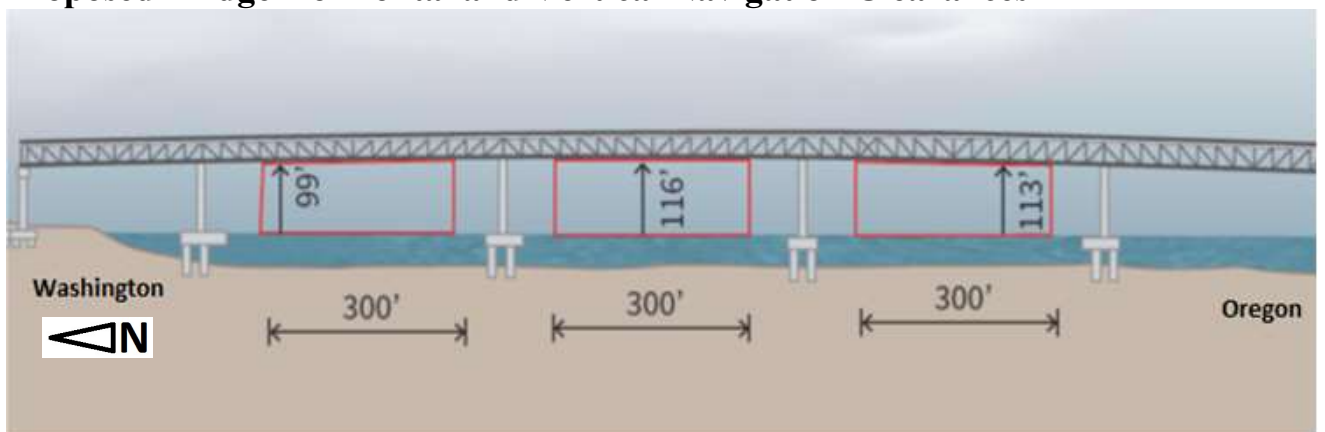
Navigation Vertical Clearance Definitions



Existing Bridge Horizontal and Vertical Navigation Clearances



Proposed Bridge Horizontal and Vertical Navigation Clearances



SOLICITATION OF COMMENTS: Mariners and maritime stakeholders are requested to express their views, in writing, on the proposed bridge and its possible impact on navigation, if any, giving sufficient detail to establish a clear understanding of their reasons for support of, or opposition to, the proposed work. To include but not limited to mariner's requirements for horizontal navigation clearances and vertical navigation clearances to include air draft and air gap requirements. The Coast Guard is particularly interested in receiving comments from maritime stakeholders with vertical navigation clearance requirements of greater than 116 feet.

The service life expectancy of any new bridge would be in excess of 100 years. Therefore maritime stakeholders are asked to consider and comment on their current navigation requirements as well as, to the best of their knowledge, their future navigation requirements for themselves and or their particular maritime industry.

FHWA and FTA have informed the USCG that they are preparing to issue a Notice of Intent to supplement an Environmental Impact Statement in the near future to review alternatives to replace the bridge at which time the public will have another opportunity to comment on the Draft Supplemental EIS.

For further information on the IBR project please refer to the following link:
<https://www.interstatebridge.org/>

Comments will be received for the record at the office of: Commander (dpw), Thirteenth Coast Guard District, 915 2nd Ave, Rm 3510, Seattle, WA or via email at <mailto:D13-SMB-D13-BRIDGES@USCG.MIL>. Comments should be sent to arrive on or before 25 April 2022.

B. J. HARRIS, Commander
Chief, Waterways Management Branch
Coast Guard District Thirteen
By direction of the District Commander
U.S. Coast Guard

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